

**HAER No. NV-15**

Logan House Road (Forest Service Road 14N33)  
Toiyabe National Forest  
Glenbrook Vicinity  
Douglas County  
Nevada

HAER  
NEV  
3 - GLENB.V,  
2 -

**PHOTOGRAPHS**

**WRITTEN HISTORICAL AND DESCRIPTIVE DATA**

**Historic American Engineering Record  
National Park Service  
Department of the Interior  
San Francisco, California**

**HISTORIC AMERICAN ENGINEERING RECORD**

**LOGAN HOUSE ROAD (Forest Service Road 14N33)**

**HAER NO: NV-15**

HAER

NED

3-GLENB.V,

2 -

**Location:**

North of U.S. 50 on the east shore of Lake Tahoe within the Lake Tahoe Basin Management Unit, Douglas County, Nevada. The road segment extends for 4.66 miles and lies at an elevation between 7190 and 8000 feet above sea level.

U.S.G.S. 7.5. minute Glenbrook, Nev., quadrangle  
Universal Transverse Mercator coordinates:  
north end-11.249830.4329930  
south end-11.248380.4326300

**Date(s) of Construction:**

Circa 1870-1880

**Engineer/Architect/Builder:**

Carson and Tahoe Lumber and Fluming Company (CTLF), incorporated in 1873 in the State of Nevada, its primary stockholders being Duane L. Bliss, H.M. Yerington, D.O. Mills, and J.A. Rigby. Bliss was president and general manager and held the office until his death in 1906.

**Present Owner (S):**

United States Department of Agriculture, Lake Tahoe Basin Management Unit.

**Present Use:**

National Forest Recreational Use/logging road.

**Significance:**

The Logan House Road represents the transportation system developed to support the massive production achievement of the CTLF in association with the Comstock Mining era of Western Nevada.

**Report Prepared By:**

Dana E. Supernowicz, Zone Historian, Lake Tahoe Basin Management Unit, 870 Emerald Bay Road, Suite 1, South Lake Tahoe, California 96150.

**Date:**

October 1994.

## **I. DESCRIPTION**

The Logan House Road is among a series of main haul logging roads developed and utilized for over 20 years by the Carson and Tahoe Lumber and Fluming Company's woods operations. The road served to connect a system of secondary, and tertiary haul roads as well as skid trails and chutes with the major wood receiving and sending station at Spooner Summit.

Like other main haul roads, the Logan House Road provided access to high elevation stands of pine and fir timber, which was either taken to Glenbrook where several sawmills were in operation, or taken as cordwood to the main Summit Camp flumes where the wood was flumed down to the Carson Valley and stored in "cordwood dumps". The wood was gathered from the "dumps" and taken initially by wagon to the Comstock mines and later by the Virginia and Truckee Railroad.

## **II. ENGINEERING INFORMATION**

From a technological standpoint, the Logan House Road and others like it, were not monumental achievements in road building industrial technology, but rather they were examples of expedient and rapid technology employed to resource extraction. It is currently described as a typical, narrow, winding, cut and fill logging road of moderate grade. The road averages 10'-12' in width and is 4.66 miles in length.

## **III. HISTORICAL INFORMATION**

Discovery of the Comstock silver lode in 1859 set the stage for three decades of intensive logging in the east central Sierra Nevada. The voracious appetite of the Virginia Range mines for timbers to shore up the shafts and cordwood to fuel the steam engines quickly devoured the available timber resources adjacent to the mining district and subsequently led to the exploitation of the timber on the slopes above Lake Tahoe.

Initially, timber harvest within the Lake Tahoe Basin was of a small scale due to the difficulty of transportation of wood products both within the basin and to the Comstock mines. In 1873, a group of business men led by D.L. Bliss and including H.M. Yerington, D.O. Mills and J.A. Rigby incorporated the Carson and Tahoe Lumber and Fluming Company (CTLF) in response to the increasing need for timber products.

The vast holdings acquired by this large company (at the height of logging in the in the 1880s, the CTLF owned over 50,000 acres of timberland) provided the economy of scale needed to efficiently harvest the timber resources within the basin. The CTLF quickly constructed a railroad to move lumber and cordwood from its mills at Glenbrook to Spooner Summit, flumes to transport products from Spooner Summit to the Carson Valley to the east, and a system of primary haul roads to move timber and cordwood from the woods to either Glenbrook or

Spooner Summit. The Logan House Road is one of these primary haul roads and was used between approximately 1870 and 1890.

The Logan House Road represents one of the greatest and most destructive episodes in the history of American logging and lumber manufacturing history. Nowhere in the United States was there such a large expenditure of capital and labor to supply the huge mines, mills, and smelters of the Comstock Lode. Virtually all the merchantable timber on the east shore of Lake Tahoe had been logged off by the 1890s. The Logan House Road and other roads of its age and class contribute to our understanding of late nineteenth century logging and road building technology.

After the decline of the Comstock mines, the Logan House Road and other Comstock Era logging roads, with the exception of seasonal grazing, saw little use until the mid-twentieth century. Since the 1950s old logging roads have been used for off highway vehicle recreation. The Logan House Road was no exception, and for the past forty years has been maintained as a two track recreation road.

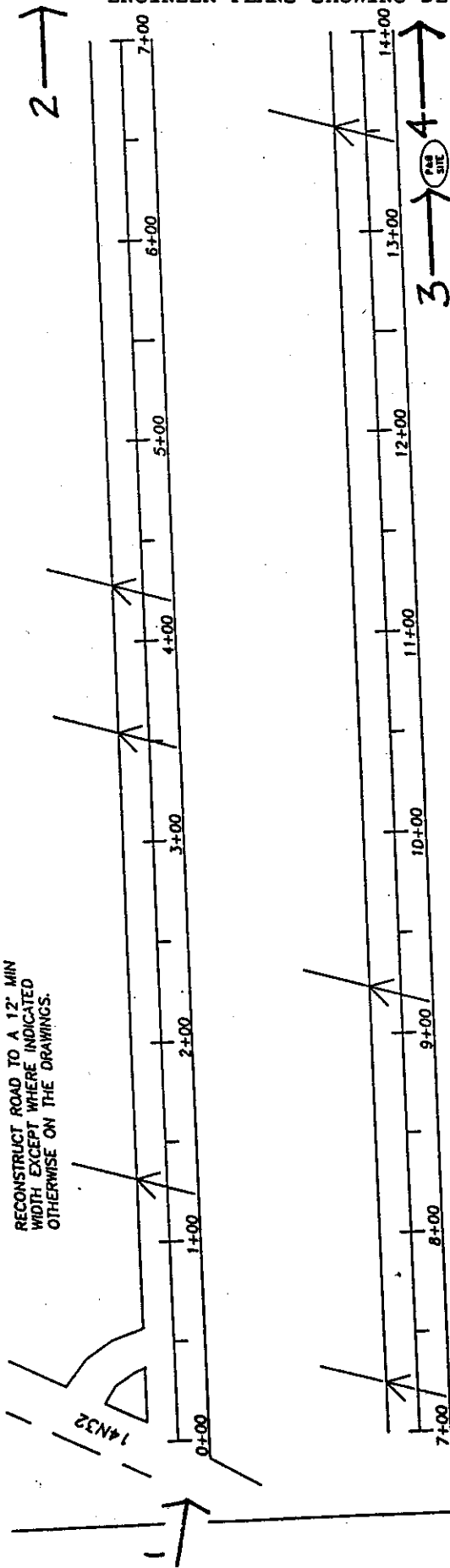
#### IV. PROJECT INFORMATION

This documentation is being prepared to fulfill the Lake Tahoe Basin Management Units (LTBMU) obligations under a Memorandum of Agreement between the LTBMU and the Nevada State Historic Preservation Officer dated July 7, 1994.

The Principle Investigator was Dana Supernowicz. The photographer was Rudy Gillard of Fortuna California.

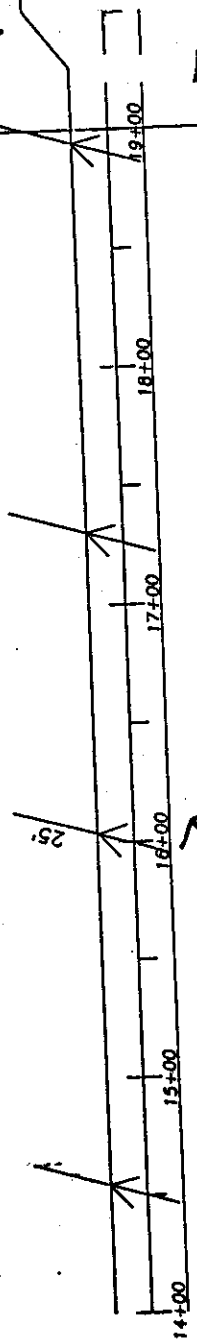
ENGINEER PLANS SHOWING DETAILS OF ALTERATIONS TO LOGAN HOUSE ROAD  
FS RD 14N33

MAINTAIN EXISTING WATERBARS  
AND LEAD OFF DITCHES  
  
RECONSTRUCT ROAD TO A 12' MIN  
WIDTH EXCEPT WHERE INDICATED  
OTHERWISE ON THE DRAWINGS.

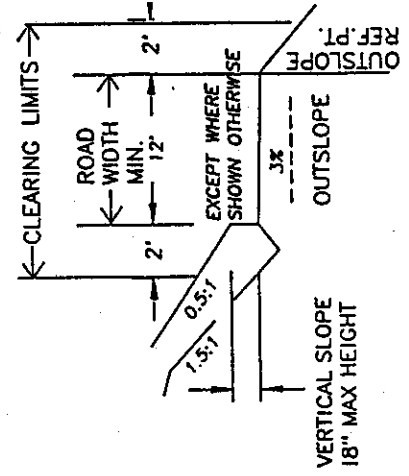


CONSTRUCT WATERBAR  
W/LEAD OFF DITCH

SEE SHEET 13  
(6,7,8)

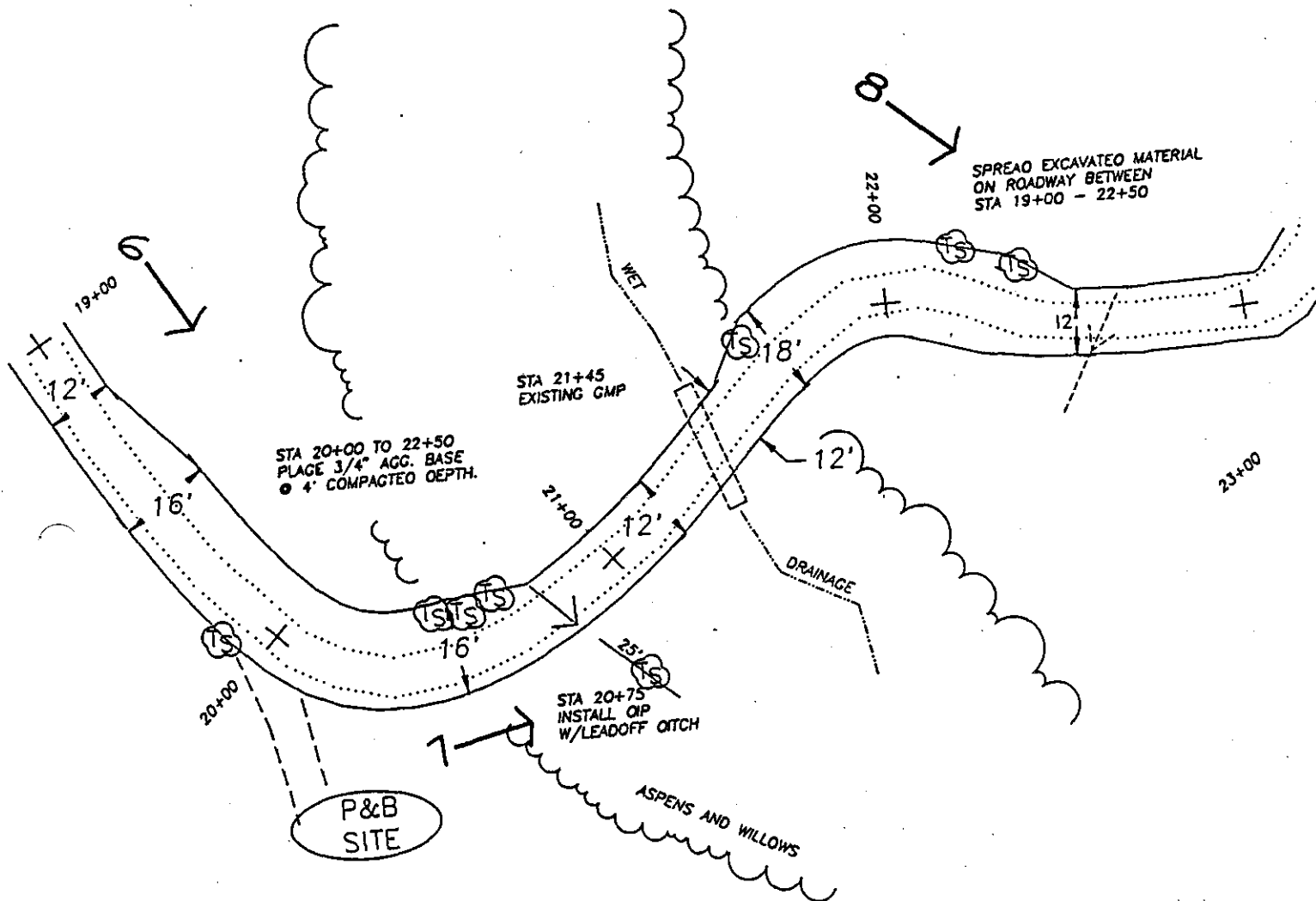


EXISTING ROAD  
VARIES 8'-10'  
WIDTH



CONCEPTUAL  
NO SCALE

ENGINEER PLANS SHOWING DETAILS OF ALTERATIONS TO LOGAN HOUSE ROAD  
FS RD 14N33

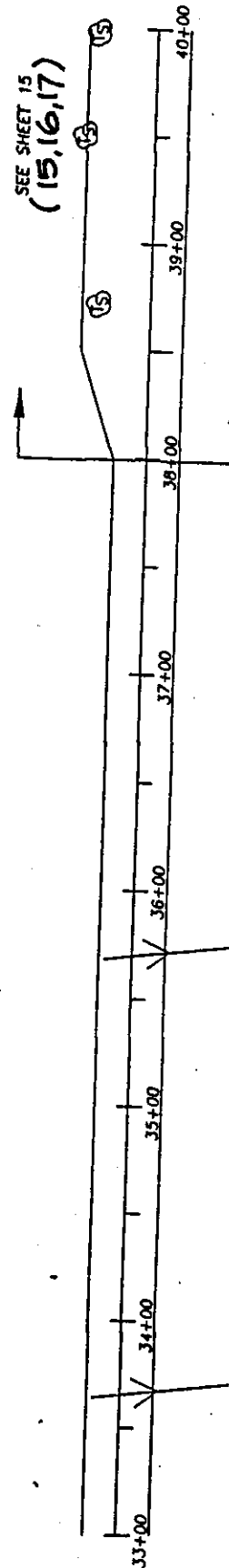
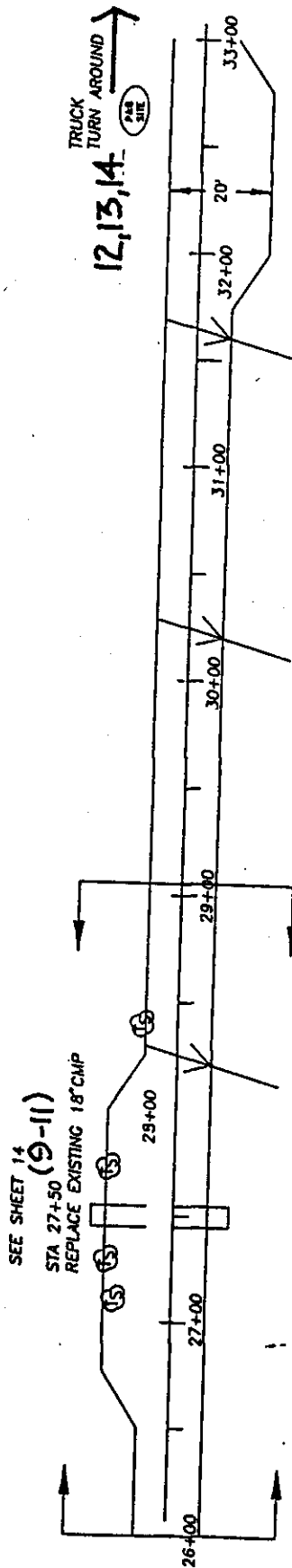
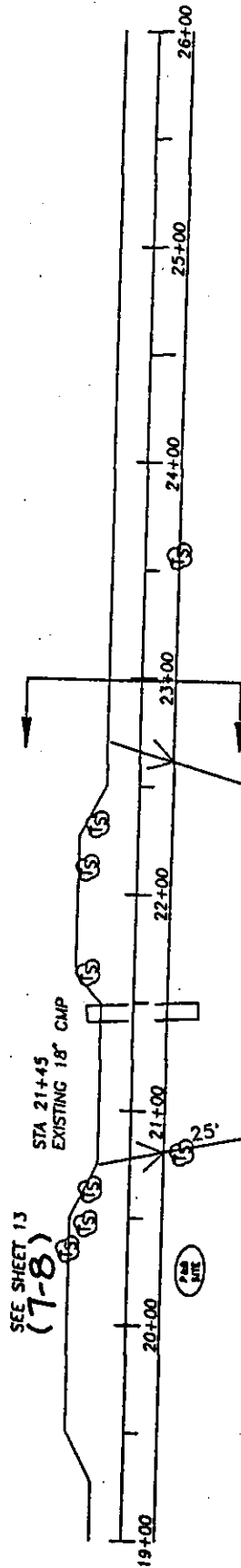


CONCEPTUAL  
NO SCALE

GENERAL NOTES:

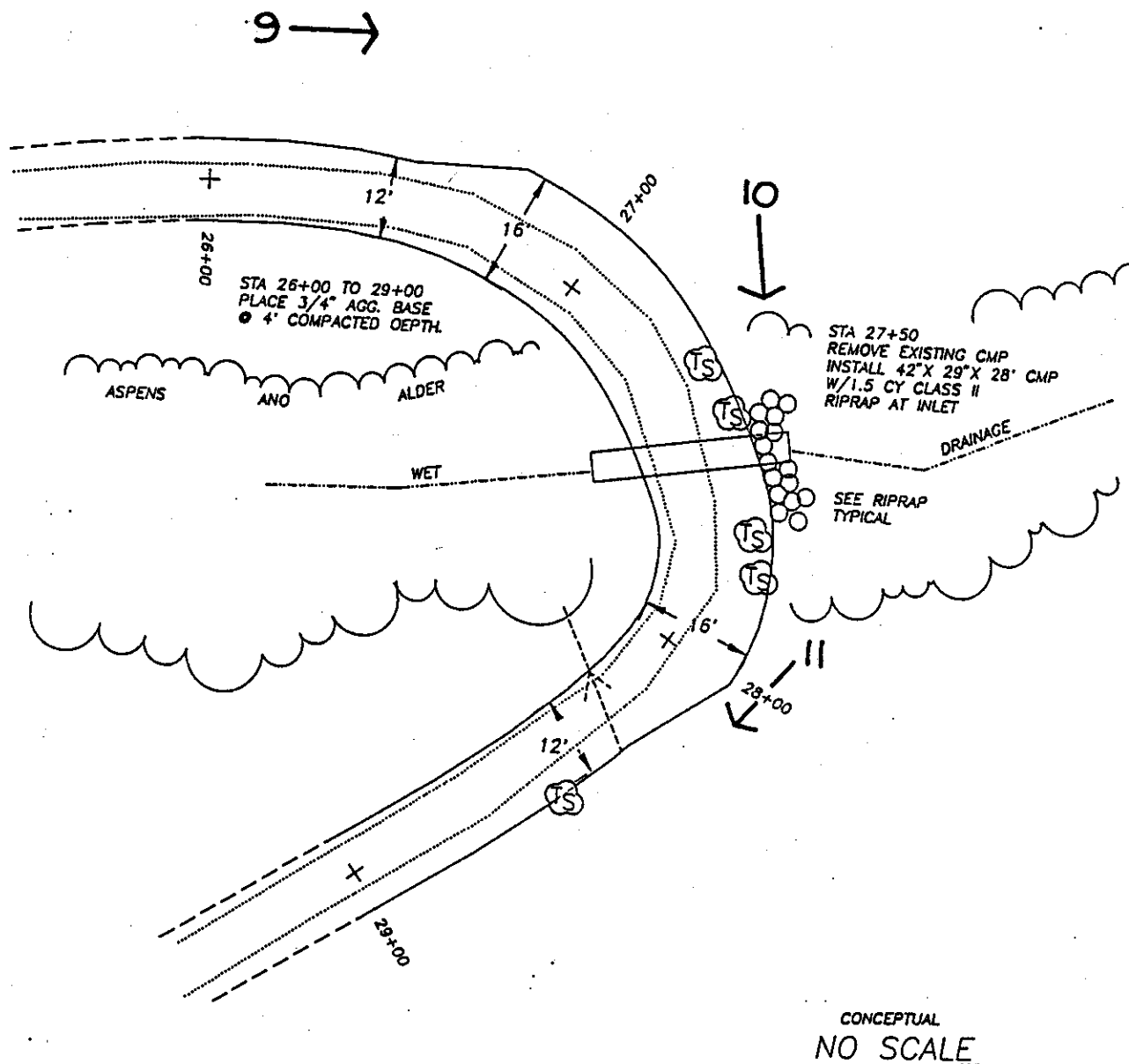
- 1- RECONSTRUCT ROAD TO 12' MINIMUM WIDTH
- 2- OUTSLOPE ROAD 3%
- 3- CONSTRUCT WATERBARS WHERE INDICATED  
MAINTAIN EXISTING WATERBARS WHERE INDICATED  
(EXISTING FEATURES ARE DASHED SYMBOLS)
- 4- STRAW MULCH ALL DISTURBED GROUND UPON  
COMPLETION OF EXCAVATION ON SEGMENT  
SHOWN ON THIS PAGE

ENGINEER PLANS SHOWING DETAILS OF ALTERATIONS TO LOGAN HOUSE ROAD  
FS RD 14N33



CONCEPTUAL  
NO SCALE

ENGINEER PLANS SHOWING DETAILS OF ALTERATIONS TO LOGAN HOUSE ROAD  
FS RD 14N33

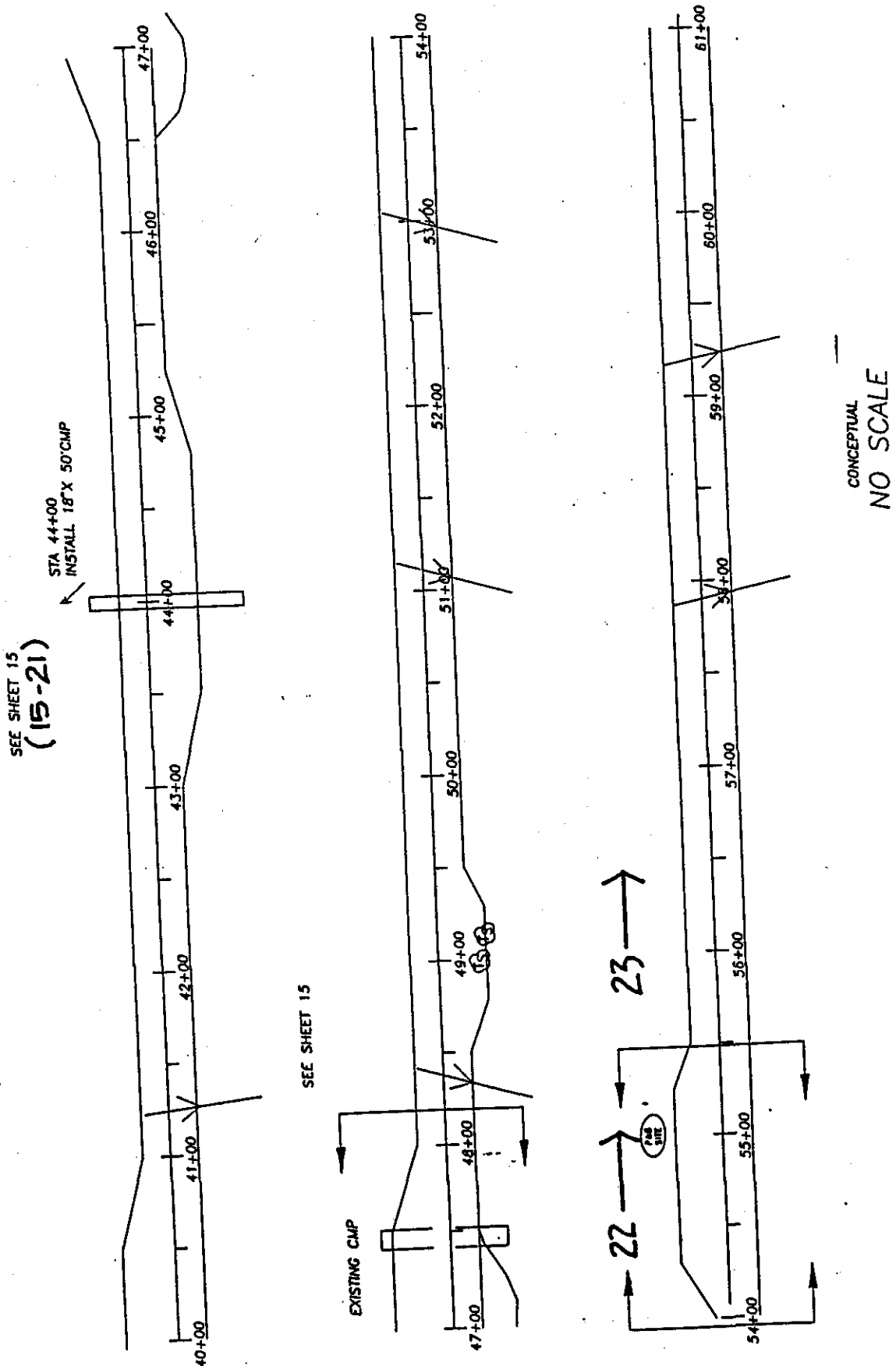


**GENERAL NOTES:**

- 1- RECONSTRUCT ROAD TO 12' MINIMUM WIDTH
- 2- OUTSLOPE ROAD 3%
- 3- THIS IS A WET ORAINAGE  
AND WILL REQUIRE DEWATERING  
PRIOR TO REMOVAL OF EXISTING  
CMP. EROSION CONTROL PLAN AND  
DEWATERING PLAN SHALL BE  
SUBMITTED TO FS FOR APPROVAL
- 4- STRAW MULCH ALL DISTURBED GROUND UPON  
COMPLETION OF EXCAVATION ON SEGMENT  
SHOWN ON THIS PAGE



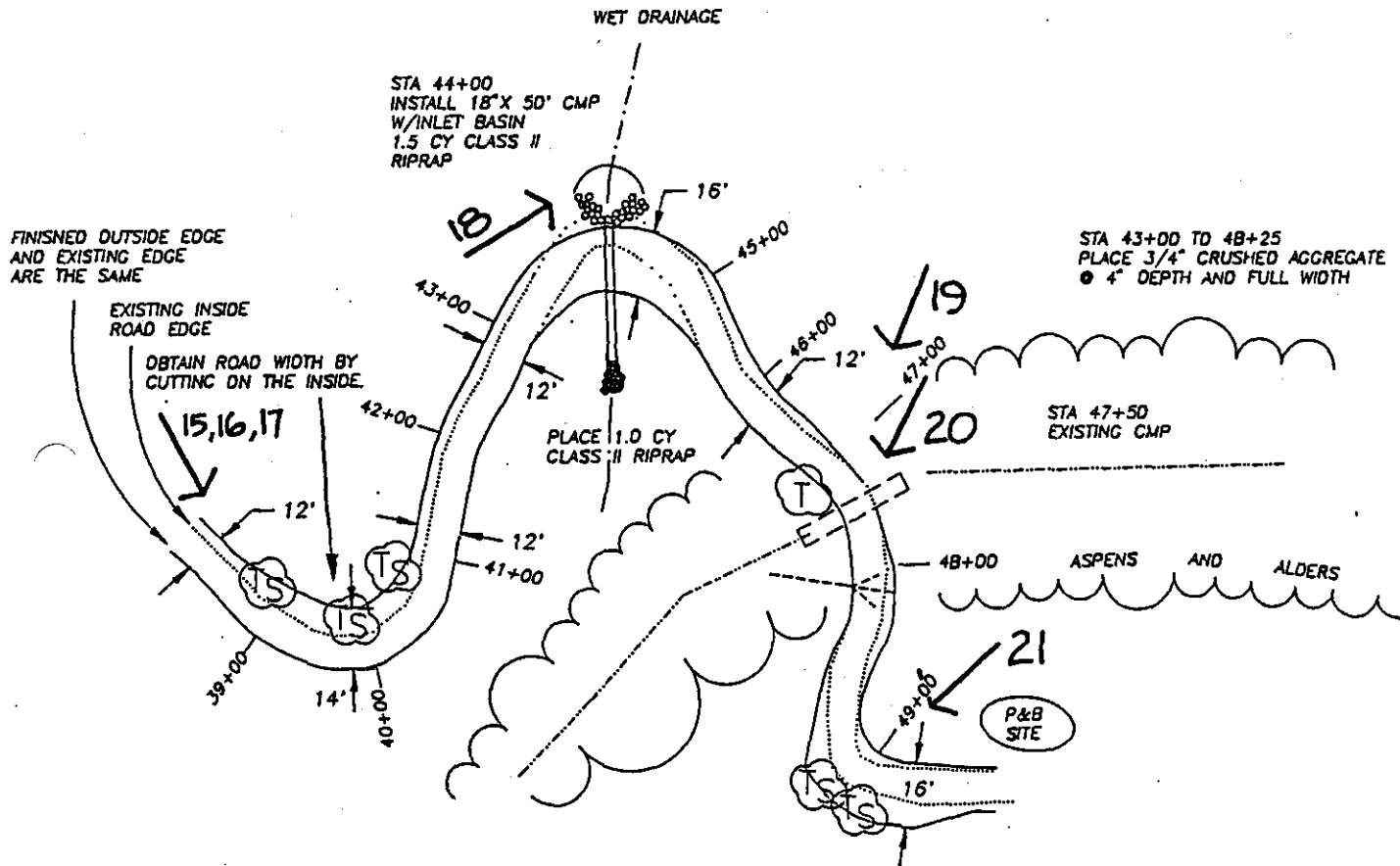
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ENGINEER PLANS SHOWING DETAILS OF ALTERATIONS TO LOGAN HOUSE ROAD  
FS RD 14N33

STA 38+00 TO 50+00  
NO SIDE CASTING PERMITTED

UTILIZE MATERIAL FROM EXCAVATION  
FROM STA 39+00 - 41+00 TO  
WIDEN ROAD AND RAISE GRADE 4'  
FROM STA 43+00 TO 45+00  
COMPACTION METHOD 4

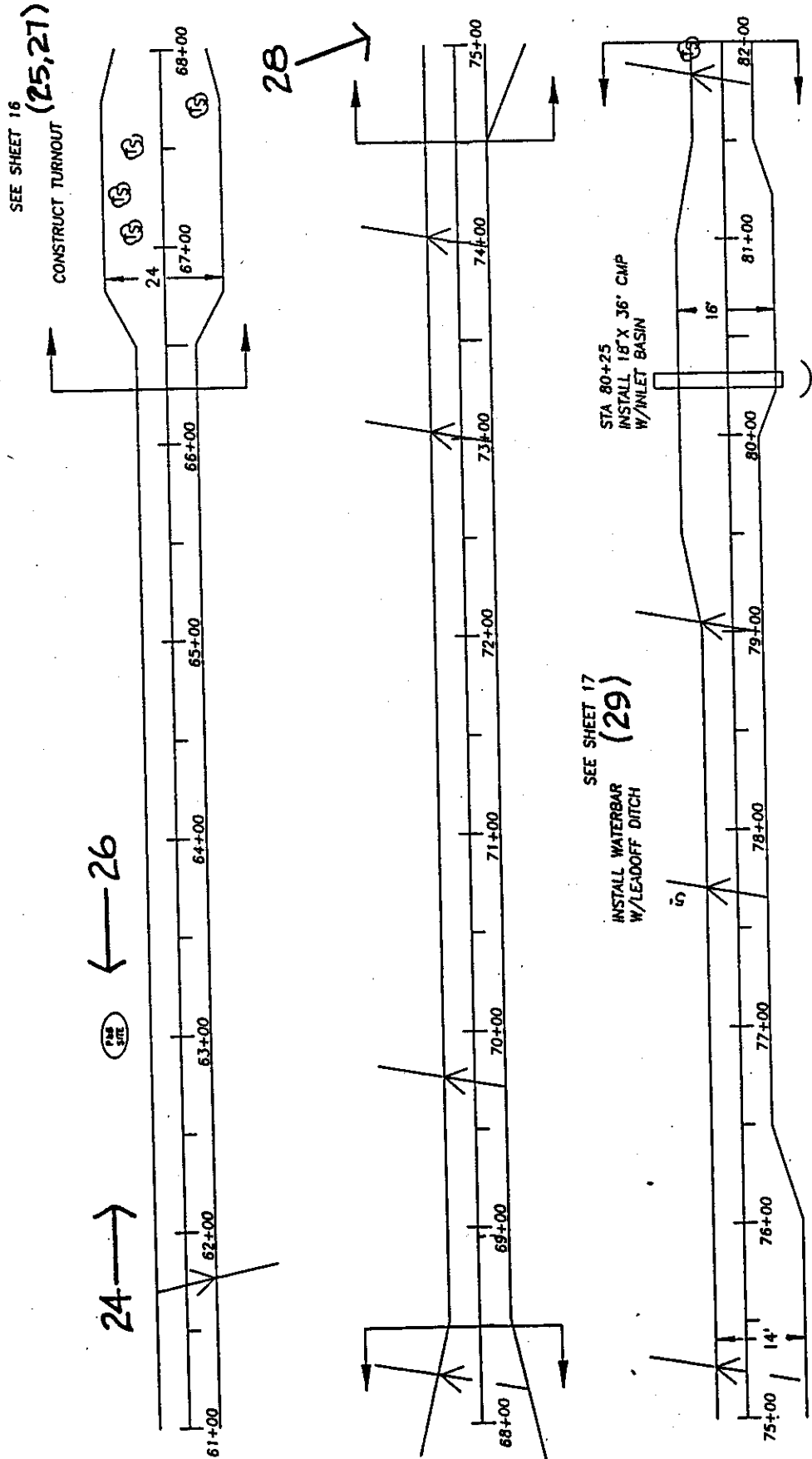


GENERAL NOTES:

- 1- RECONSTRUCT ROAD TO 12' MINIMUM WIDTH
- 2- OUTSLOPE ROAD 3%
- 3- THIS IS A WET DRAINAGE AND WILL REQUIRE DEWATERING PRIOR TO REMOVAL OF EXISTING CMP. EROSION CONTROL PLAN AND DEWATERING PLAN SHALL BE SUBMITTED TO FS FOR APPROVAL
- 4- STRAW MULCH ALL DISTURBED GROUND UPON COMPLETION OF EXCAVATION ON SEGMENT SHOWN ON THIS PAGE.

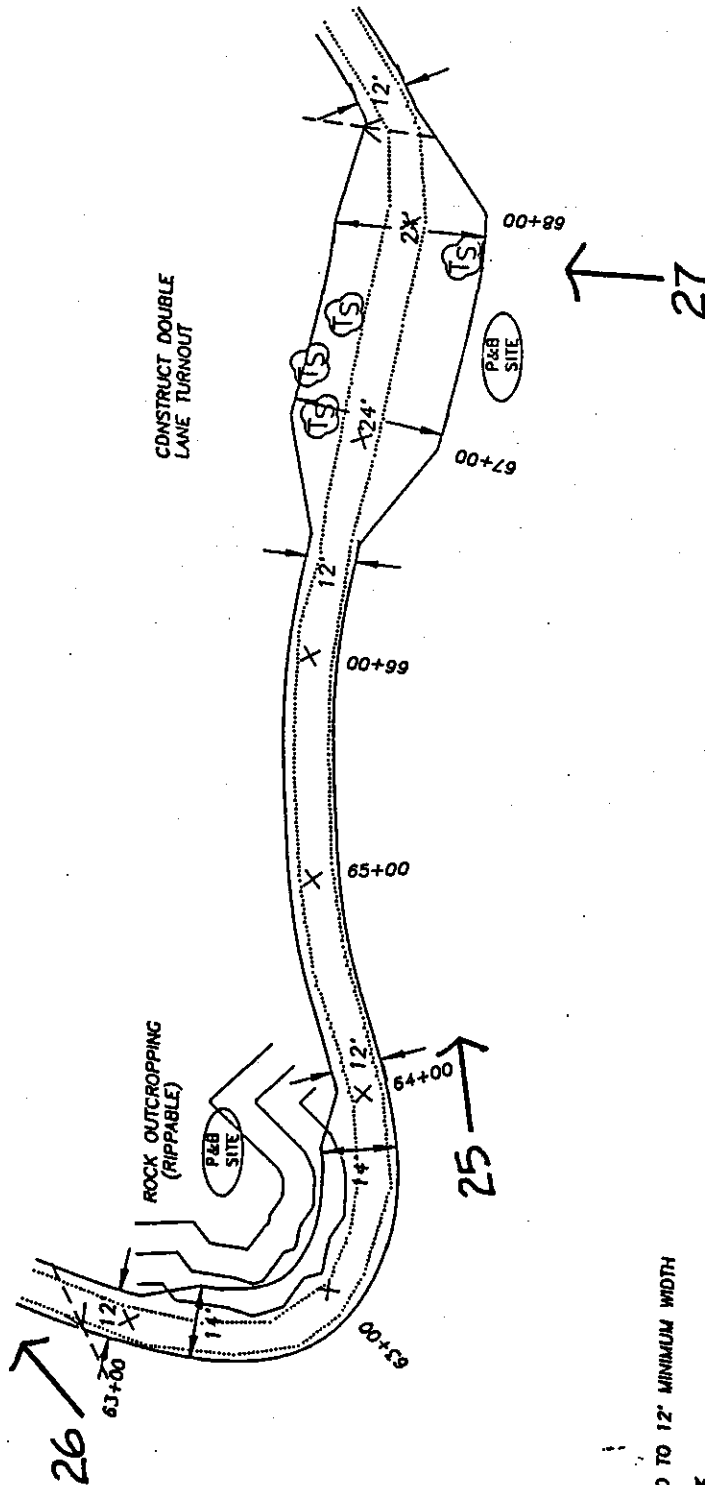
CONCEPTUAL  
NO SCALE

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CONCEPTUAL  
NO SCALE

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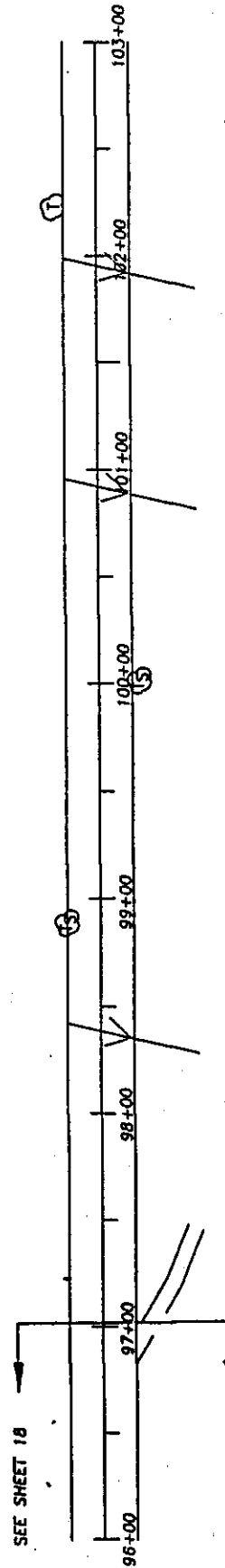
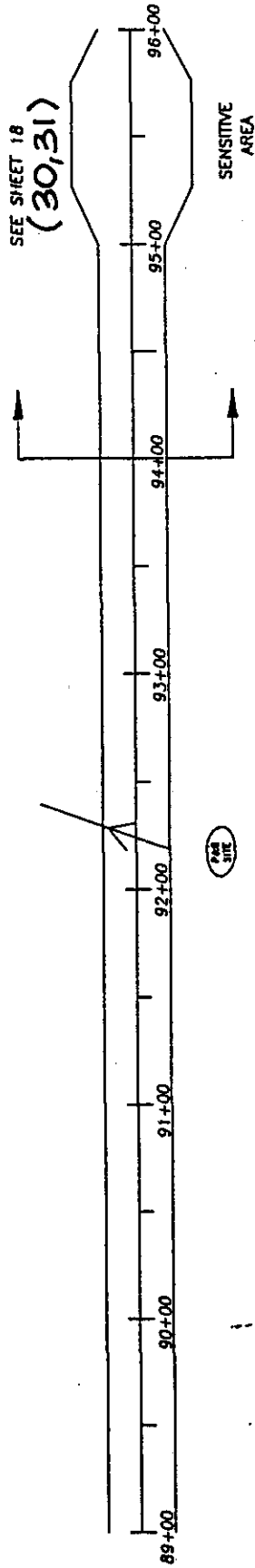
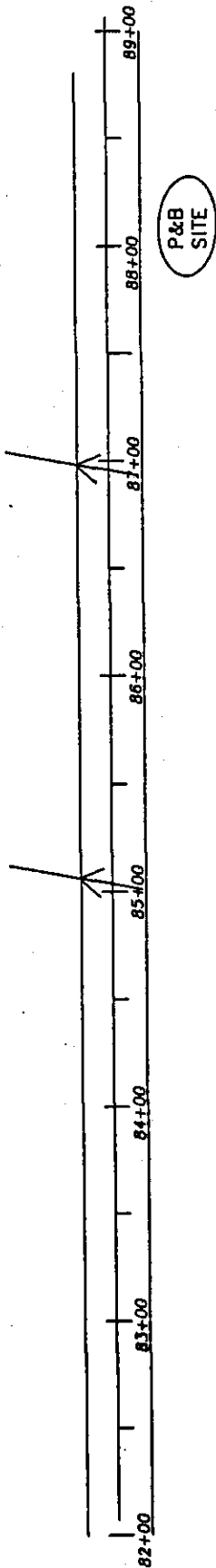
- 1- RECONSTRUCT ROAD TO 12' MINIMUM WIDTH
- 2- DUTSLOPE ROAD 3%
- 3- CONSTRUCT WATERBARS WHERE INDICATED  
MAINTAIN EXISTING WATERBARS WHERE INDICATED  
(EXISTING FEATURES ARE DASHED SYMBOLS)

CONCEPTUAL

NO SCALE

ENGINEER PLANS SHOWING DETAILS OF ALTERATIONS TO LOGAN HOUSE ROAD  
FS RD 14N33

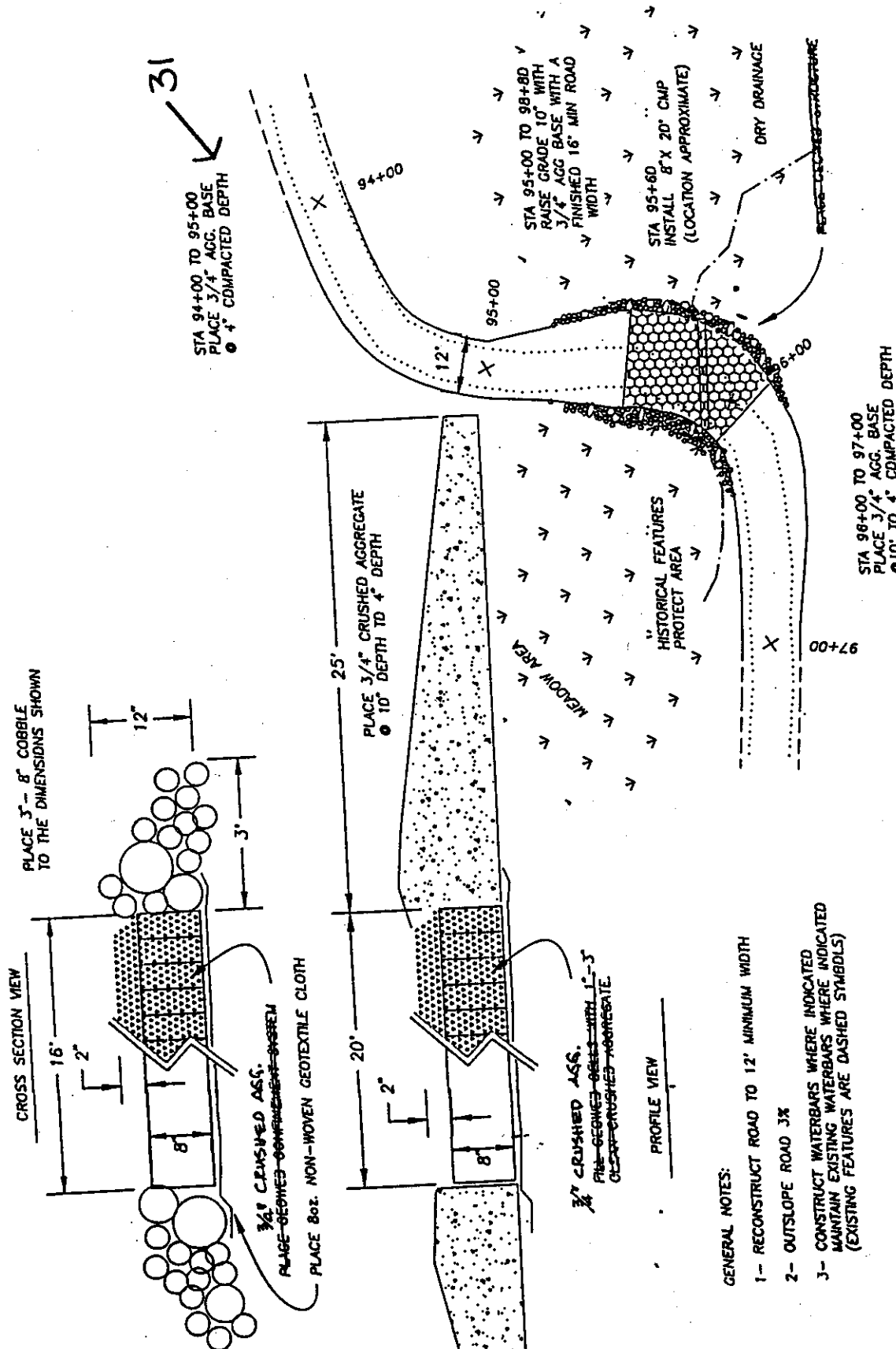
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FS RD 14N33



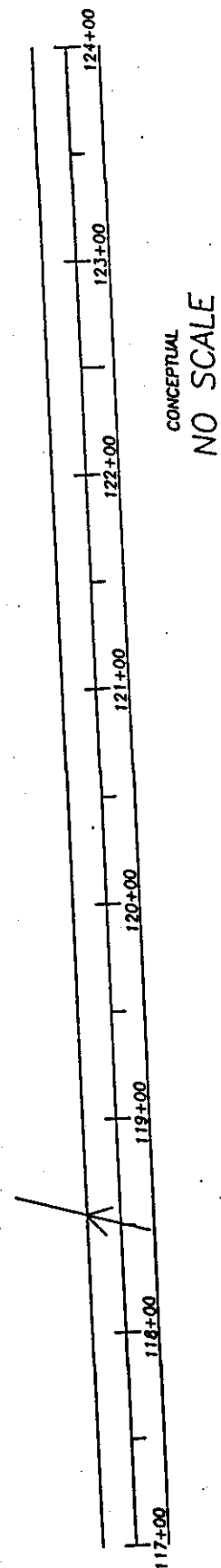
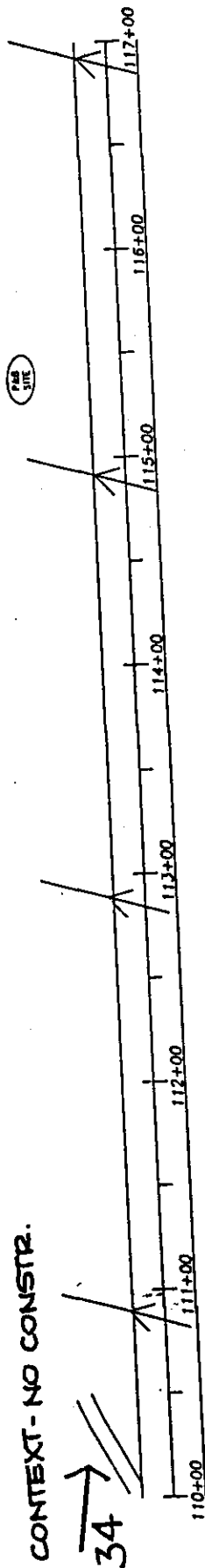
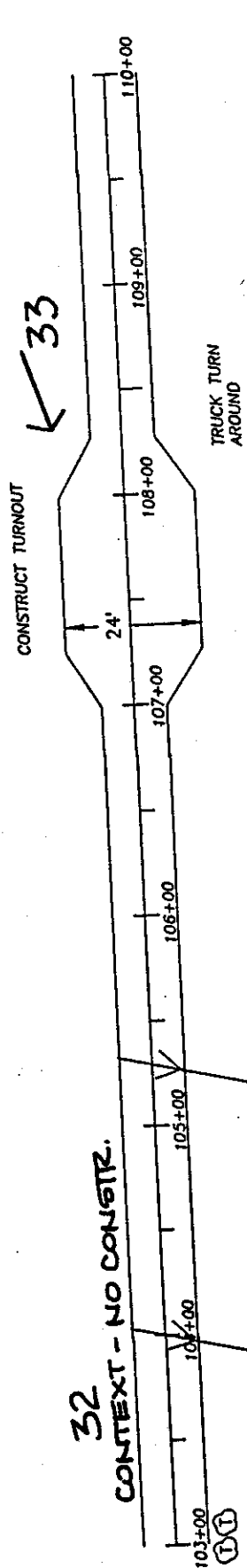
CONCEPTUAL  
NO SCALE

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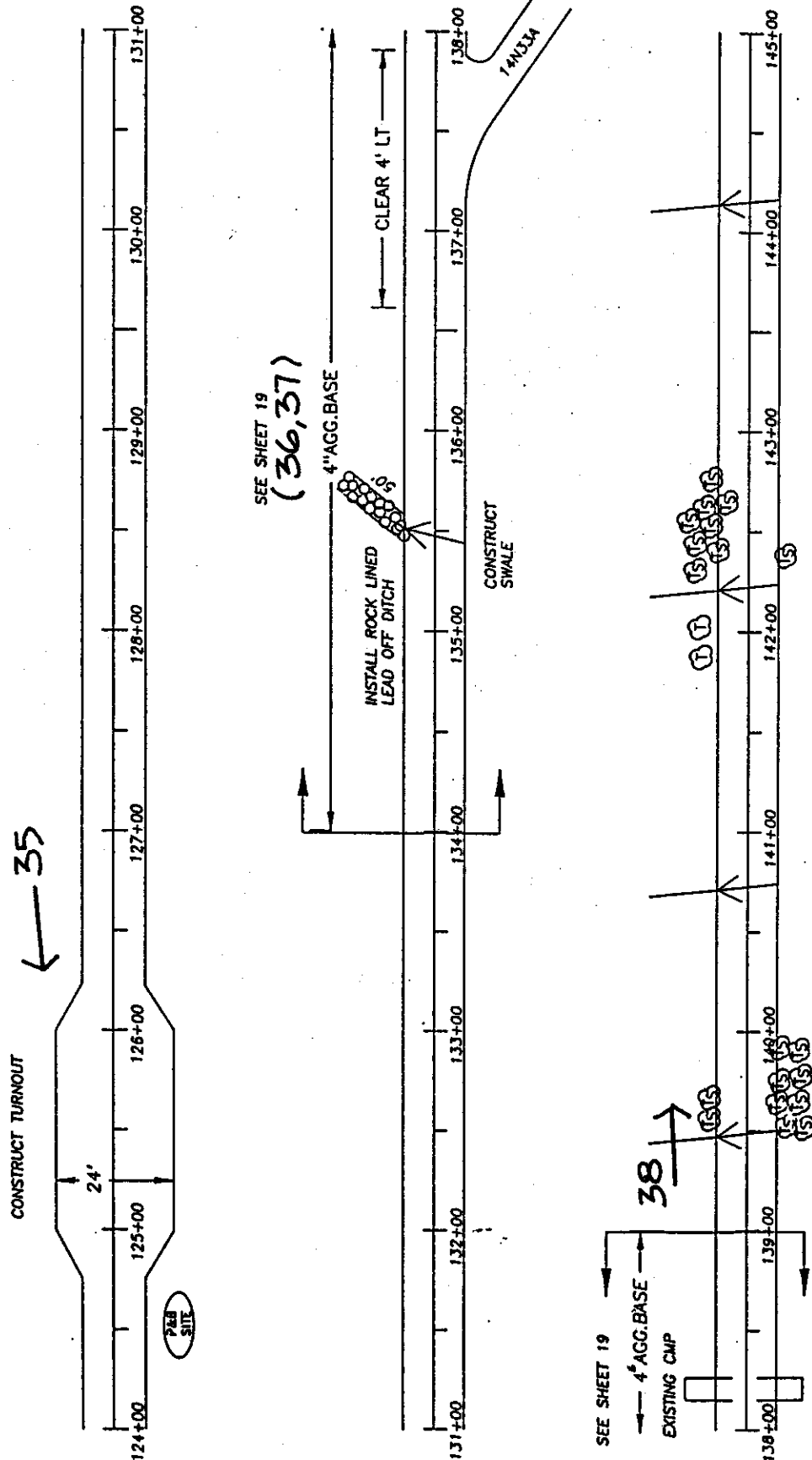


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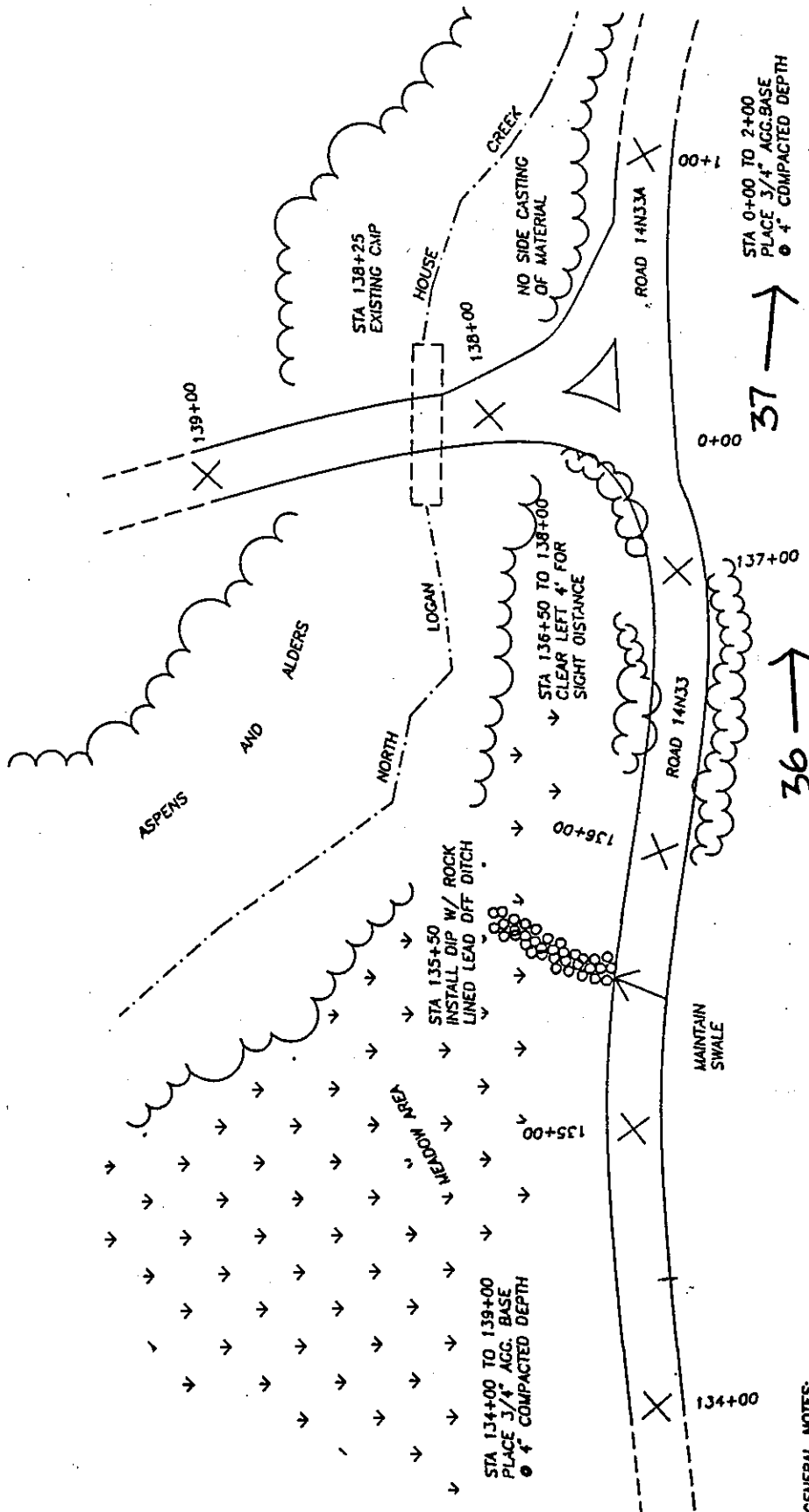




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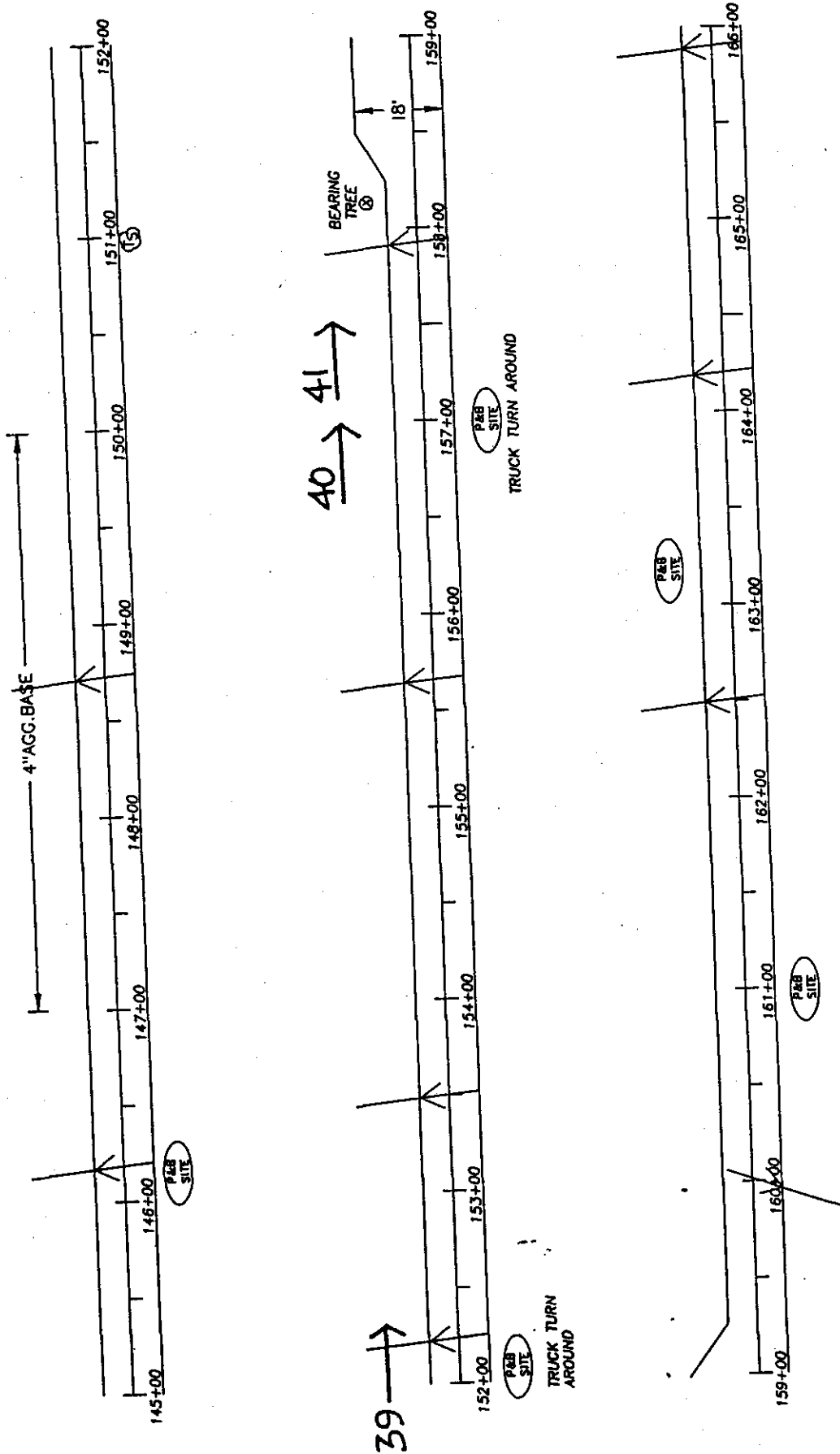


GENERAL NOTES:

- 1- RECONSTRUCT ROAD TO 12' MINIMUM WIDTH
- 2- OUTSLOPE ROAD 3:1
- 3- CONSTRUCT WATERBARS WHERE INDICATED  
MAINTAIN EXISTING WATERBARS WHERE INDICATED  
(EXISTING FEATURES ARE DASHED SYMBOLS)
- 4- STRAW MULCH ALL DISTURBED GROUND UPON  
COMPLETION OF EXCAVATION ON SEGMENT  
SHOWN ON THIS PAGE.

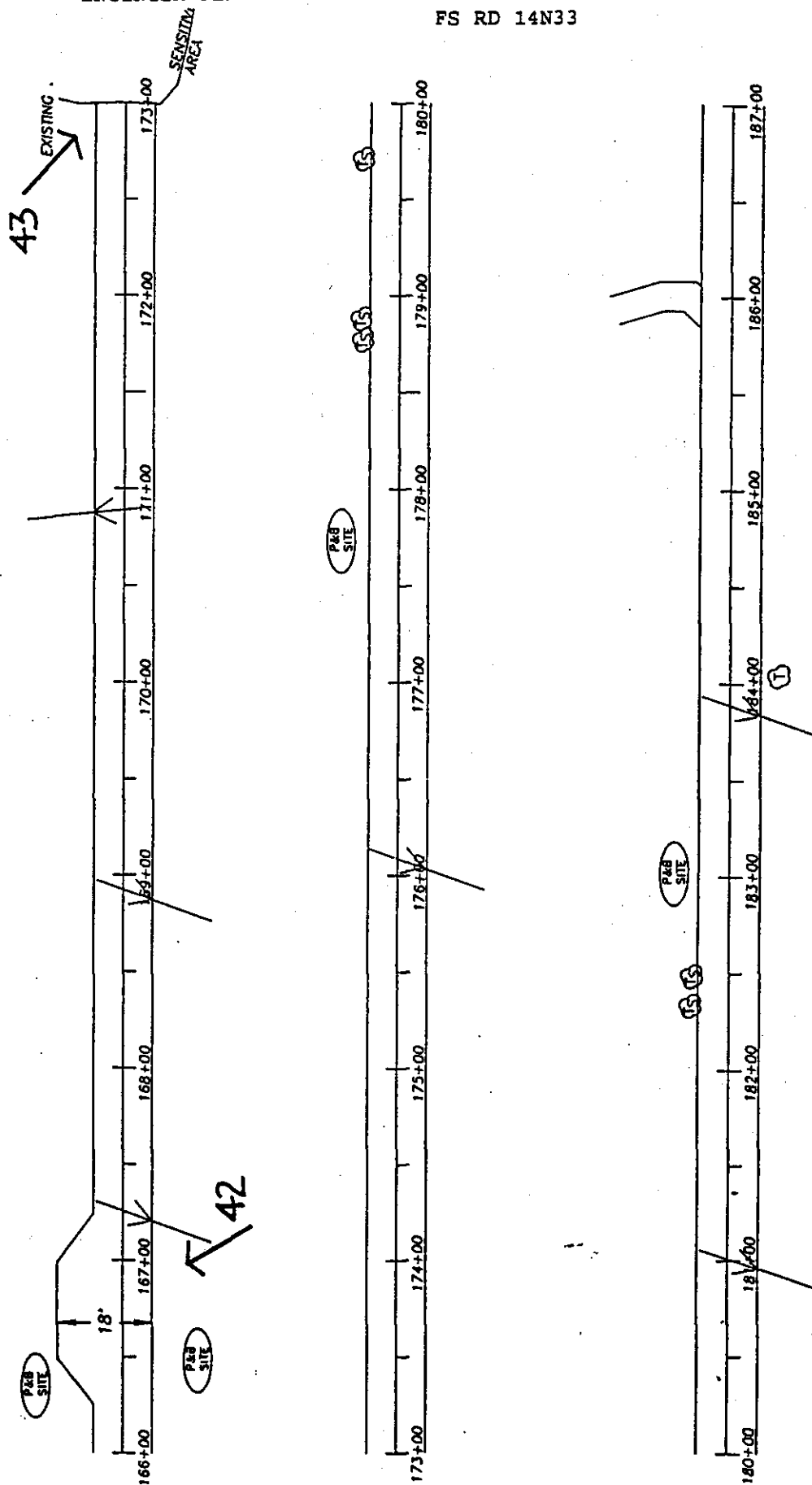
CONCEPTUAL  
NO SCALE

ENGINEER PLANS SHOWING DETAILS OF ALTERATIONS TO LOGAN HOUSE ROAD



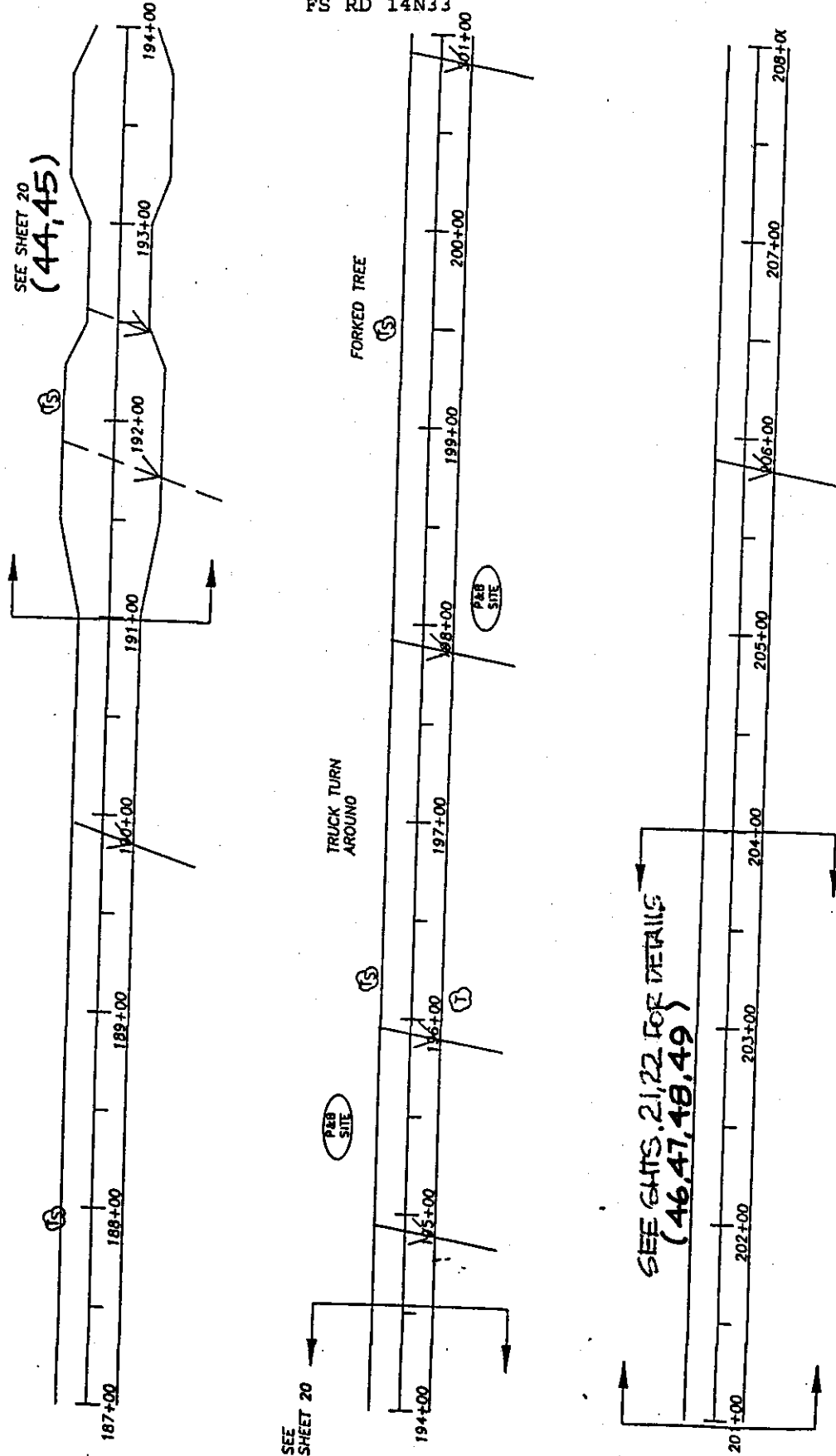
CONCEPTUAL  
NO SCALE

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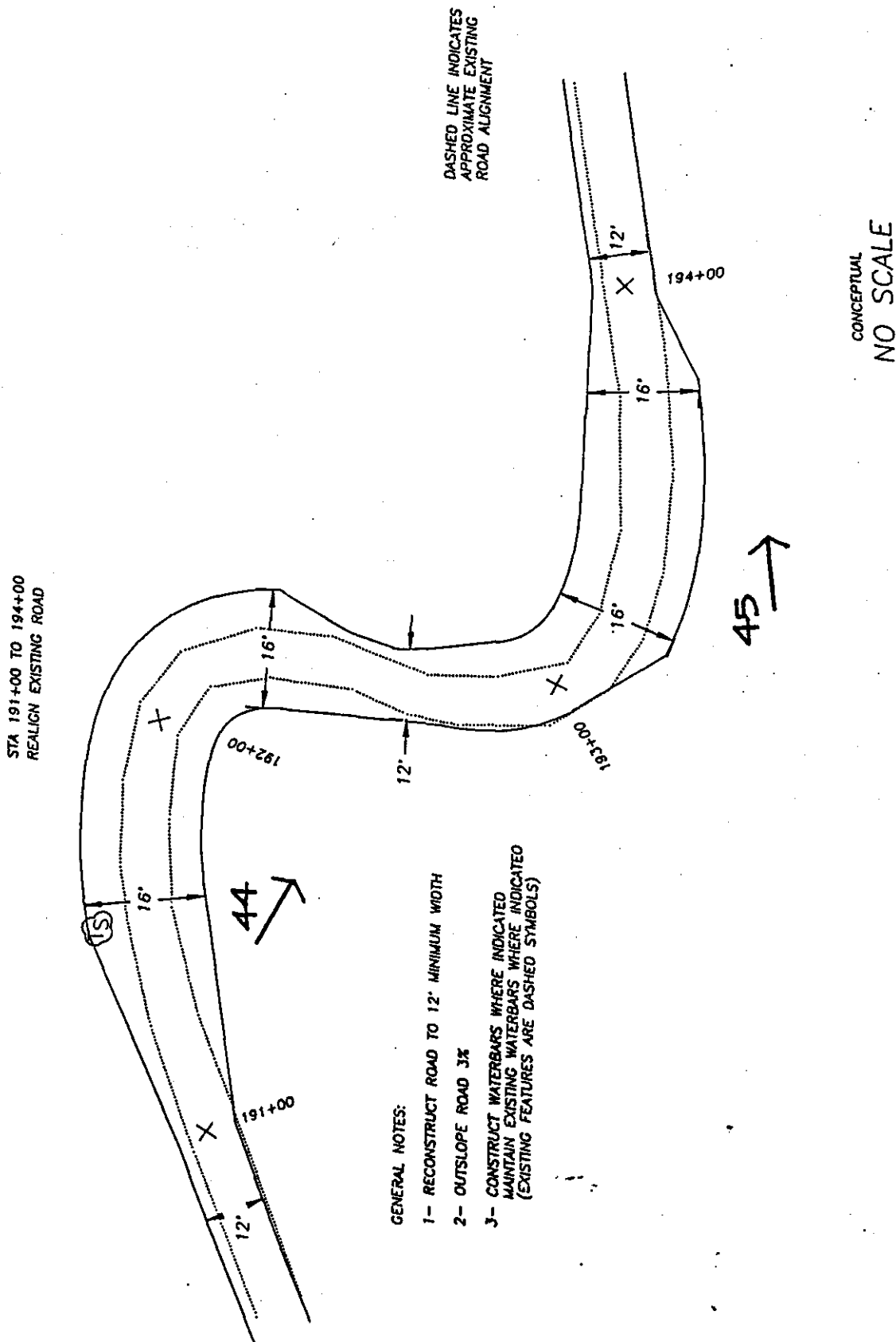
CONCEPTUAL  
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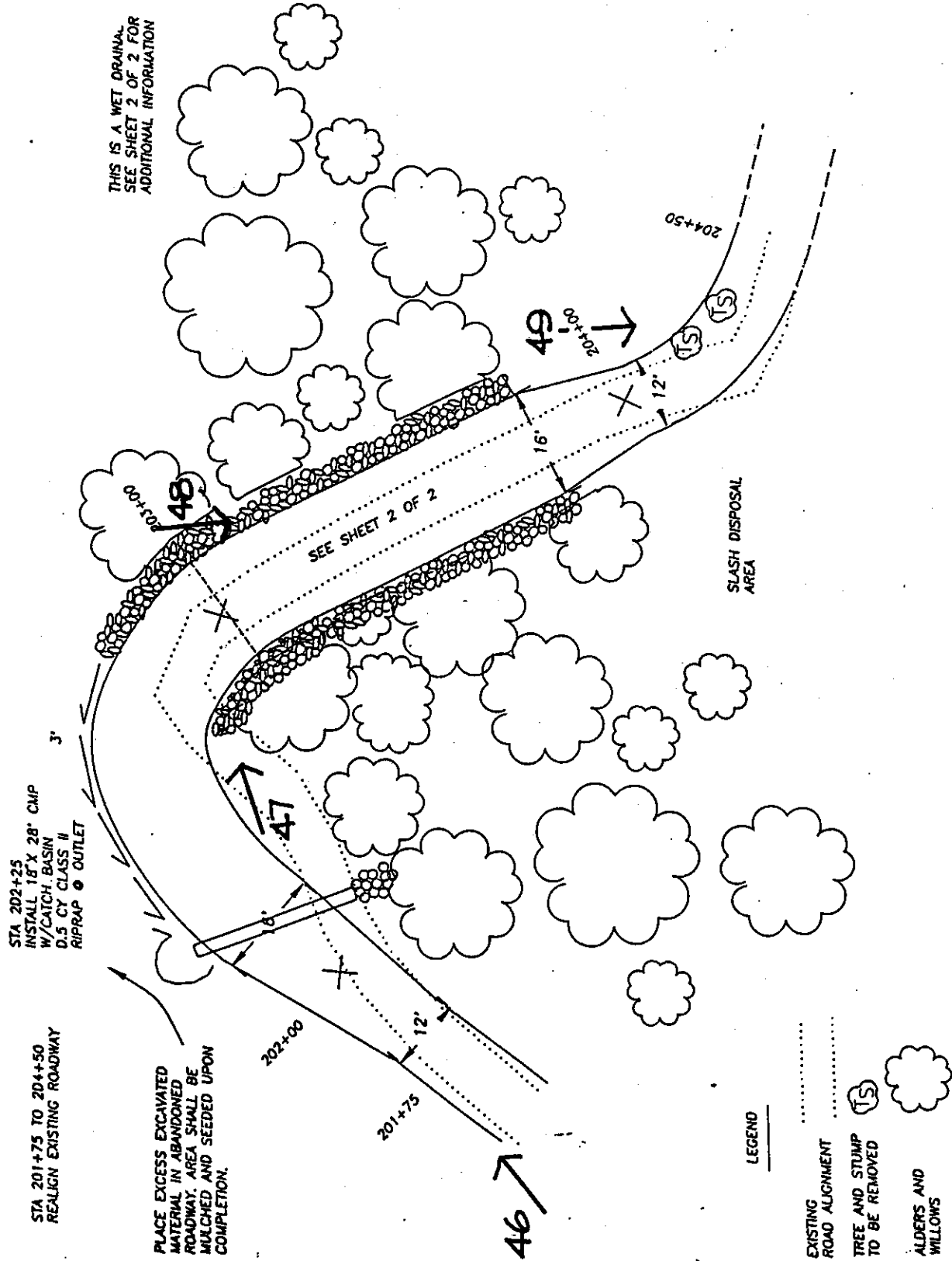


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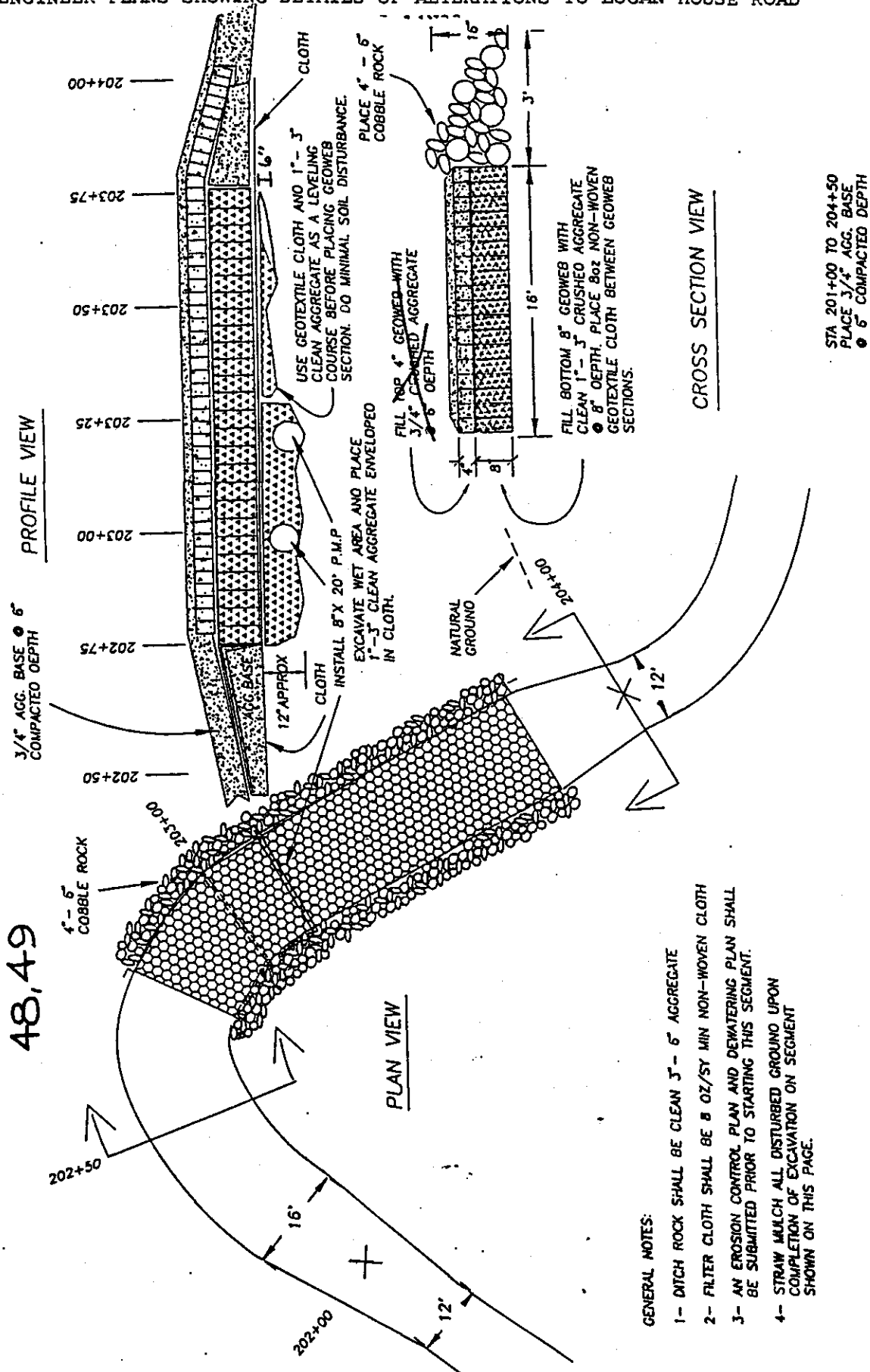
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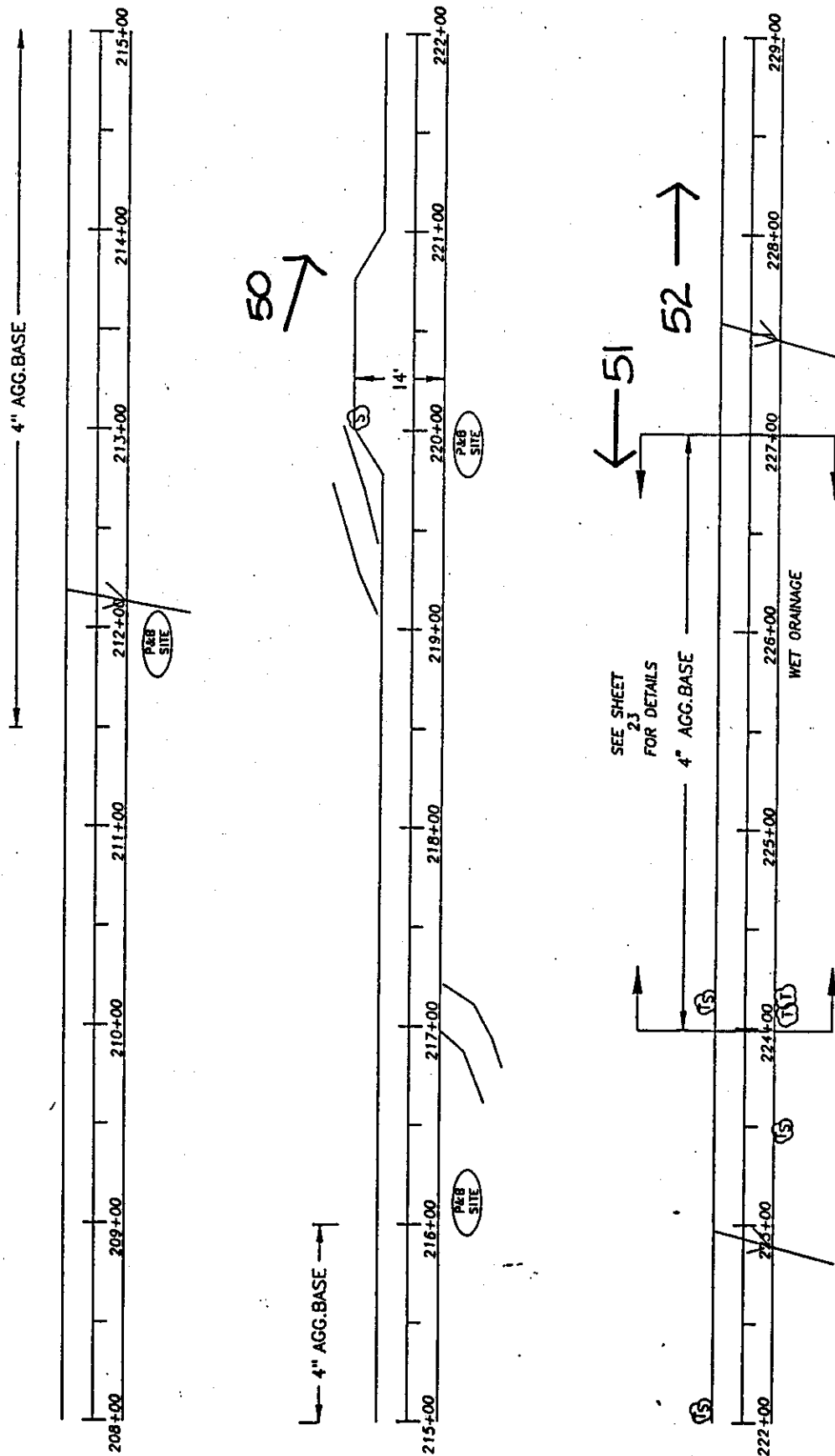
ENGINEER PLANS SHOWING DETAILS OF ALTERATIONS TO LOGAN HOUSE ROAD



STA 201+00 TO 204+50  
PLACE 3/4" AGG. BASE  
• 6" COMPACTED DEPTH



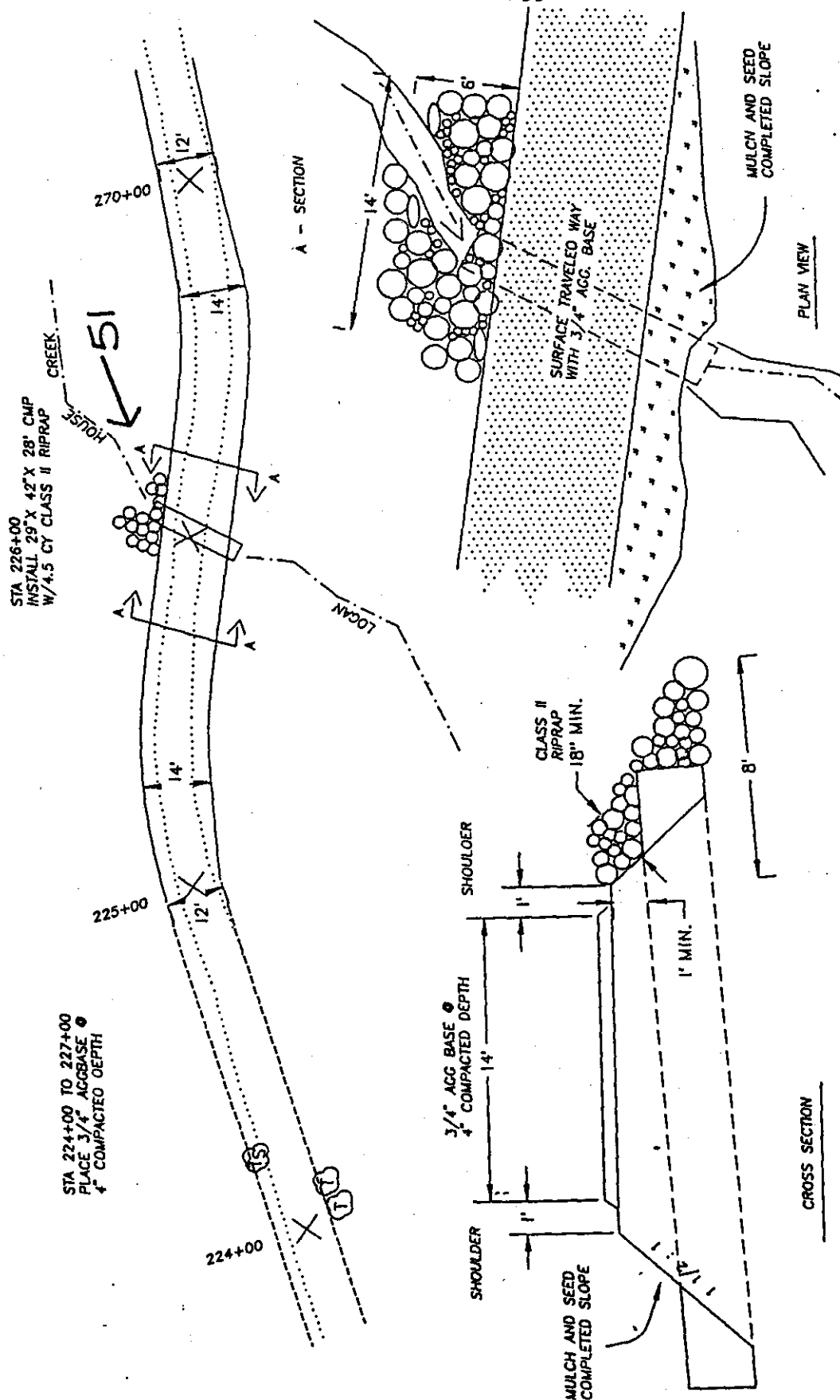
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FS RD. 14N33

NOTE:

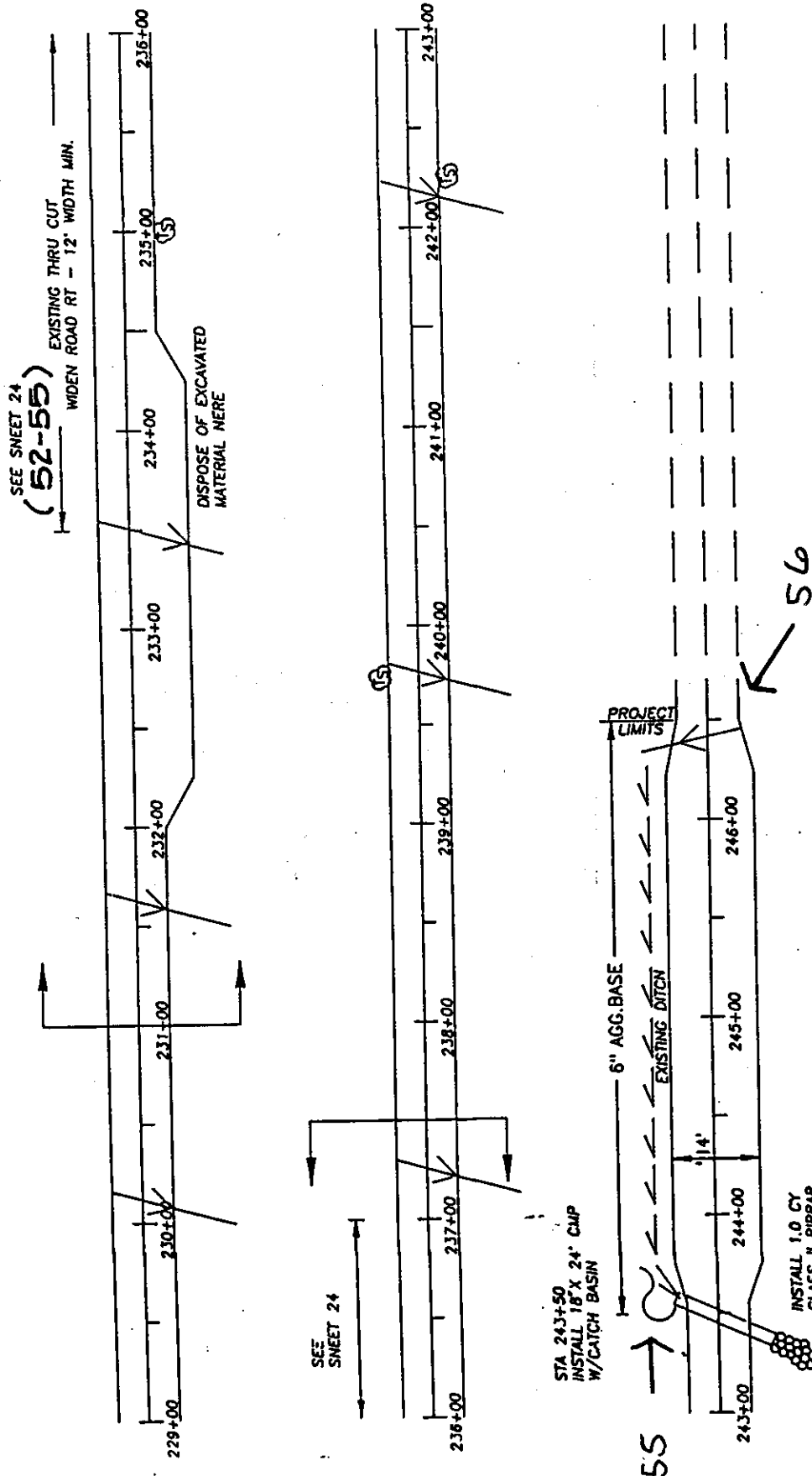
AN EROSION CONTROL PLAN AND A  
DEWATERING PLAN SHALL BE SUBMITTED  
FOR APPROVAL PRIOR TO STARTING  
THIS SEGMENT.



CONCEPTUAL DRAWING  
NO SCALE



ENGINEER PLANS SHOWING DETAILS OF ALTERATIONS TO LOGAN HOUSE ROAD  
FS RD 14N33



CONCEPTUAL  
NO SCALE

